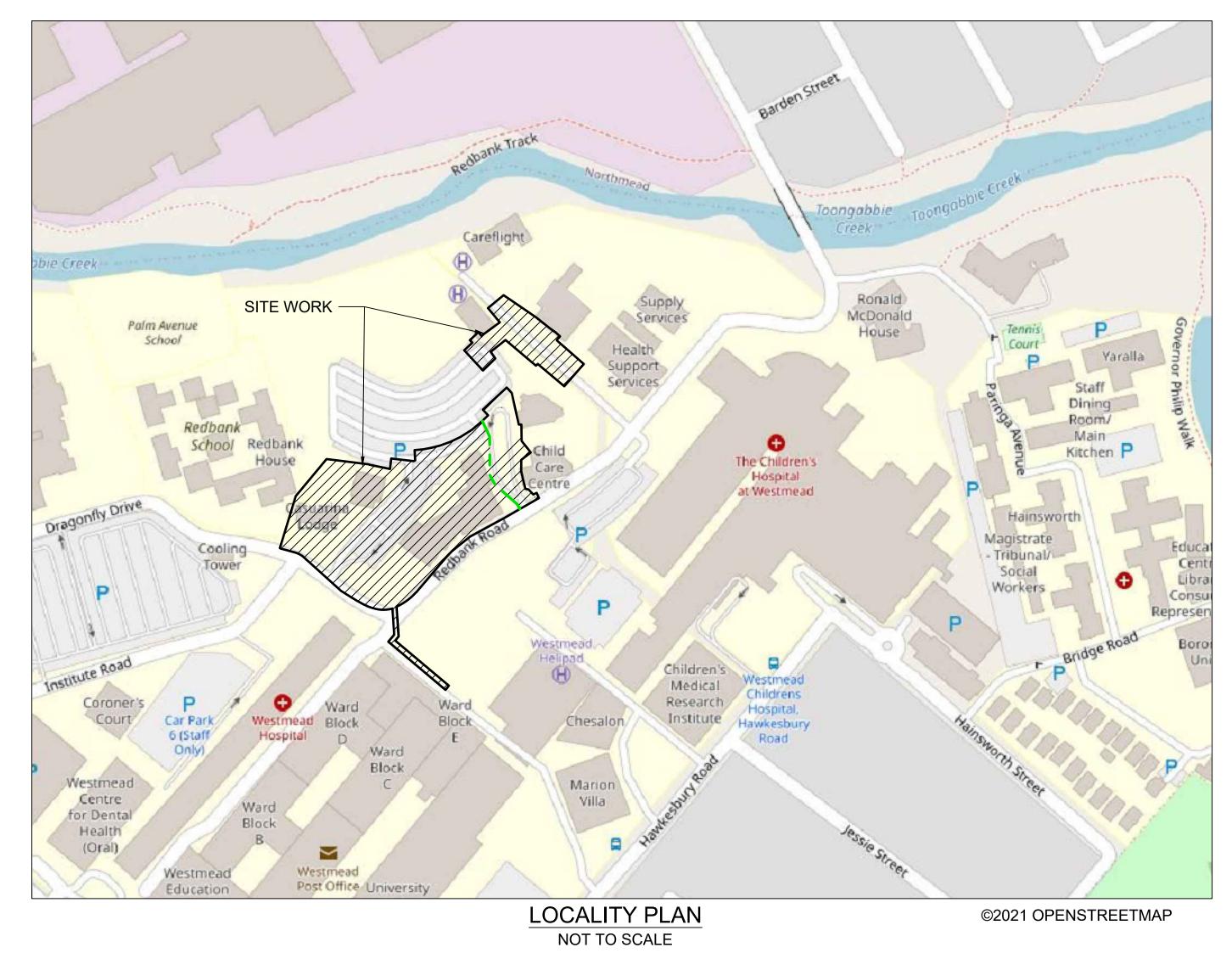
# CUMBERLAND WEST MENTAL HEALTH SERVICES RELOCATION

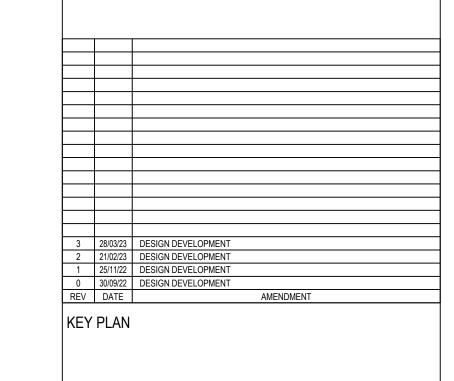
INTEGRATED MENTAL HEALTH COMPLEX (IMHC) EARLY WORKS - CIVIL WORKS DESIGN DEVELOPMENT

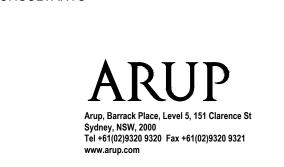




DRAWING No.	DRAWING TITLE
IMHC-CV-DG-2001	COVER SHEET, LOCALITY PLAN AND DRAWING REGISTER
IMHC-CV-DG-2006	GENERAL NOTES - SHEET 1 OF 2
IMHC-CV-DG-2007	GENERAL NOTES - SHEET 2 OF 2
IMHC-CV-DG-2011	GENERAL LEGEND
IMHC-CV-DG-2100	SEDIMENT AND EROSION CONTROL - PLAN
IMHC-CV-DG-2191	SEDIMENT AND EROSION CONTROL - DETAILS
IMHC-CV-DG-2200	BULK EARTHWORKS - OVERALL SITE - PLAN
IMHC-CV-DG-2301	CIVIL WORKS - PLAN - SHEET 1 OF 2
IMHC-CV-DG-2302	CIVIL WORKS - PLAN - SHEET 2 OF 2
IMHC-CV-DG-2601	STORMWATER - PLAN - SHEET 1 OF 2
IMHC-CV-DG-2602	STORMWATER - PLAN - SHEET 2 OF 2
IMHC-CV-DG-2700	PAVEMENT - OVERALL SITE - PLAN
IMHC-CV-DG-2791	PAVEMENT - PROFILE
IMHC-CV-DG-2800	EXISTING UTILITIES - OVERALL SITE - PLAN
IMHC-CV-DG-2806	UTILITIES DIVERSION COORDINATION - PLAN













PROJECT **WESTMEAD INTEGRATED MENTAL HEALTH COMPLEX (IMHC) EARLY WORKS CIVIL WORKS** 

PROJECT NO. 281916-00

DRAWING TITLE **COVER SHEET, LOCALITY PLAN AND DRAWING REGISTER** 

**DESIGN DEVELOPMENT** DT

SCALE @ B1 NOT TO SCALE

DRAWING NO. REVISION IMHC-CV-DG-2001

APPROVED

#### **GENERAL**

- 1. ALL WORKS SHALL BE IN ACCORDANCE WITH CITY OF PARRAMATTA "DEVELOP CONTROL PLAN 2011", CITY OF PARRAMATTA COUNCIL "DEVELOPMENT ENGINEERING DESIGN GUIDELINES" AND HEALTH INFRASTRUCTURE "ENGINEERING SERVICE GUIDELINES".
- 2. SHOULD ANY AMBIGUITY, ERROR, OMISSION, DISCREPANCY, INCONSISTENCY OR OTHER FAULT EXIST OR SEEM TO EXIST IN THE DOCUMENTS, IMMEDIATELY NOTIFY THE ENGINEER OR SUPERINTENDENT.
- 3. ALL WORKS ON WATERFRONT LAND AS DEFINED BY THE WATER MANAGEMENT ACT 2000 MUST BE IN ACCORDANCE WITH THE NSW NATURAL RESOURCES ACCESS REGULATOR (NRAR GUIDELINES) FOR CONTROLLED ACTIVITIES ON WATERFRONT LAND.
- 4. EARLY AND MAIN WORKS CONTRACTORS TO REVIEW ALL IMHC DRAWINGS TO ENSURE COORDINATION WITH EACHOTHER.

#### **SURVEY**

- 1. ALL SURVEY FOR THIS DESIGN IS SUPPLIED BY: LTS LOCKLEY (REFERENCE 32572088DT TO GDA94) SUITE 1, LEVEL 1, 810 PACIFIC HIGHWAY, GORDON, NSW, 2072 PHONE 1300 587 000 FAX 02 9499 7760
- 2. ALL BEARINGS AND DISTANCES BY SURVEY.

#### **EROSION AND SEDIMENT**

- 1. ALL EROSION AND SEDIMENT CONTROL MEASURES TO BE IN ACCORDANCE WITH LANDCOM "SOILS AND CONSTRUCTION MANUAL VOLUME 1, MARCH 2004".
- 2. WORKS SHALL BE UNDERTAKEN IN THE FOLLOWING SEQUENCE: A) INSTALL EROSION AND SEDIMENT CONTROLS.
- B) STRIP AND STOCKPILE TOPSOIL AND CARRY OUT ALL BULK EARTHWORKS.
- C) TOPSOIL AND REHABILITATE BULK EARTHWORK AREAS IMMEDIATELY UPON COMPLETION.
- D) UNDERTAKE REMAINING SITE WORKS IN ACCORDANCE WITH THE ENGINEERING PLANS.
- E) REMOVE SOIL AND WATER MANAGEMENT WORKS NOT REQUIRED FOR OTHER STAGES OF CONSTRUCTION ONCE UPSTREAM SURFACES ARE STABILISED TO THE SATISFACTION OF CITY OF PARRAMATTA COUNCIL
- CONTROLS AFFECTED BY WORKS ARE TO BE RE-ESTABLISHED PRIOR TO THE COMPLETION OF EACH DAYS WORK.
- 4. DUST CONTROL MEASURES SHALL BE IMPLEMENTED CONTINUOUSLY DURING CONSTRUCTION WORKS THROUGH REGULAR WATERING TO THE SATISFACTION OF THE PRINCIPAL'S AUTHORISED PERSON (PAP).
- 5. THE CONTRACTOR IS TO STABILISE TOPSOIL STOCKPILE AND BARE AREAS AS SOON AS THEY REACH FINAL LEVELS. STABILISATION TO BE BY HYDROSEEDING OR OTHER METHOD APPROVED BY CITY OF PARRAMATTA. ALL SEEDED AREAS TO BE WATERED TWICE WEEKLY UNTIL GRASS IS ESTABLISHED OR COVERED WITH BITUMEN STRAW MULCH.
- 6. THE CONTRACTOR SHALL TEMPORARILY REHABILITATE ANY DISTURBED AREAS WITHIN 14 DAYS. WHERE FINAL SHAPING HAS OCCURRED THE CONTRACTOR SHALL PROVIDE FINAL REHABILITATION WITHIN 7 DAYS.
- 7. NO MORE THAN 50 m OF TRENCH IS TO BE OPEN AT ANY ONE TIME.
- 8. AREAS OVER STORMWATER AND SEWER LINES NOT IN ROADS TO BE MULCHED AND SEEDED AS SOON AS POSSIBLE BUT NO LATER THAN WITHIN 14 DAYS AFTER BACKFILL
- 9. AREAS OVER ELECTRICITY POWER, TELEPHONE AND GAS SUPPLY TRENCHES NOT IN ROADS ARE TO BE SEEDED AND MULCHED AS SOON AS POSSIBLE BUT NO LATER THAN WITHIN 14 DAYS AFTER BACKFILL.
- 10. ALL TEMPORARY EARTH BERMS, DIVERSION AND SEDIMENT BASIN EMBANKMENTS ARE TO BE TRACK ROLLED, SEEDED OR MULCHED FOR TEMPORARY VEGETATION COVER AS SOON AS THEY HAVE BEEN FORMED.

#### SEDIMENT CONTROL MEASURES

1. DURING EARTHWORKS, CAR PARK WORKS AND ROADWORKS. TEMPORARY DIVERSION BANKS SHOULD BE CONSTRUCTED TO LIMIT SLOPE LENGTH. WHERE POSSIBLE. IN ACCORDANCE WITH THE **FOLLOWING:** 

RECOMMENDED MAXIMUM	SPACING BETWEEN CROSS
BANKS ON	ALL ROADS
SLOPE	MAXIMUM SPACING (m)
0 TO 1%	150
1 TO 3%	100
3 TO 5%	70
5 TO 10%	50
10 TO 17%	16

- 2. ALL EXISTING STORMWATER PITS TO BE COVERED OR PROTECTED BY SEDIMENT CONTROL MEASURES AS ILLUSTRATED IN THE ENGINEERING DRAWINGS. NEWLY CONSTRUCTED DRAINAGE INLET PITS SHALL ALSO BE PROTECTED IMMEDIATELY AFTER INSTALLATION.
- 3. SEDIMENT TRAPS ARE TO BE MAINTAINED SUCH THAT: A) SEDIMENT IS REMOVED SUCH THAT NO LESS THAN 70% OF THE
- DESIGN CAPACITY REMAINS AT ANY ONE TIME. B) MATERIALS ARE REPLACED OR REPAIRED AS REQUIRED TO ENSURE SERVICEABILITY OF BOTH THE ELEMENT AND THE TRAP.

- 4. PERMANENT DRAINAGE STRUCTURES INCLUDING PIPES AND PITS ARE TO BE HANDED OVER IN A CLEAN CONDITION AT THE COMPLETION OF THE CONTRACT MAINTENANCE PERIOD.
- 5. FOLLOWING COMPLETION AND RESTORATION OF SITE. REMOVE ALL MATERIALS AND FILL DIVERSION DRAINS. WATERWAYS AND SEDIMENT TRAPS. COMPACT IN ACCORDANCE WITH SPECIFICATION TO MATCH LEVELS OF THE PREVIOUSLY COMPLETED WORKS. PROVIDE 150 mm TOPSOIL AND HYDROSEED.
- 6. AN ACCESS POINT TO ALLOW MACHINE ENTRY / EXIT ARE TO INCLUDE A ROUNDED EARTH MOUND 0.3 m HIGH WITH 10H:1V BATTERS
- 7. THE CONTRACTOR SHALL PROVIDE A 0.4 m WIDE TURF STRIP BEHIND ALL KERB AND GUTTER AT COMPLETION OF FOOTPATH FORMATION AND 1.0 m WIDE AROUND ALL SURFACE INLET PITS
- 8. THE CONTRACTOR SHALL MAINTAIN A LOG BOOK DETAILING RECORDS OF ALL RAINFALL
- CONDITION OF SOIL AND WATER MANAGEMENT STRUCTURES - ANY ADDITIONAL REMEDIAL WORKS REQUIRED THE LOG BOOK SHALL BE MAINTAINED ON A DAILY BASIS AND BE
- MADE AVAILABLE TO ANY AUTHORISED PERSON UPON REQUEST. THE ORIGINAL LOG BOOK SHALL BE ISSUED TO THE PROJECT MANAGER AT THE COMPLETION OF THE WORKS.
- 9. THE CONTRACTOR SHALL AT ALL TIMES RESTRICT CONSTRUCTION EQUIPMENT MOVEMENT TO THE ESSENTIAL CONSTRUCTION AREAS. THE CONTRACTOR SHALL NOT EXTEND LAND DISTURBANCE BEYOND 2 m FROM THE EDGE OF ANY ESSENTIAL CONSTRUCTION ACTIVITY.
- 10. THE CONTRACTOR SHALL PROVIDE CATCH DRAINS AT THE BOTTOM OF ALL BATTERS AND DIVERT THE CATCH DRAINS AND ANY TAIL OUT DRAINS TO DRAIN TO THE NEAREST STORMWATER PIT.

#### CLEARING AND GRUBBING

- 1. THE WORK SITE IS TO BE CLEARED OF ALL ORGANIC MATTER, RUBBISH OR OTHERWISE UNSUITABLE MATERIALS.
- 2. ALL TREES, OTHER THAN THOSE IDENTIFIED AND APPROVED FOR REMOVAL BY THE PROJECT ARBORIST. SHALL BE RETAINED.
- 3. GRUBBING OUT OF STUMPS AND ROOTS IS REQUIRED TO A MIN DEPTH OF 500 mm BELOW EXISTING GROUND LEVEL. FOLLOWING GRUBBING. ANY VOIDS OR DEPRESSIONS ARE TO BE FILLED WITH SELECTED MATERIAL AND COMPACTED IN 150 mm LAYERS TO THE DENSITY OF THE SURROUNDING UNDISTURBED MATERIAL
- 4. ALL MATERIAL DERIVED FROM CLEARING AND GRUBBING IS TO BE DISPOSED OF BY THE CONTRACTOR AT AN APPROVED DISPOSAL SITE.

#### **DEMOLITION**

- 1. ALL STRUCTURAL DEMOLITION WORKS TO BE UNDERTAKEN IN ACCORDANCE WITH REQUIREMENTS OF AS 2601 AND STRUCTURAL **ENGINEERING SPECIFICATIONS.**
- 2. HAZARDOUS MATERIALS (INCLUDING ASBESTOS) ARE TO BE HANDLED, STORED, TREATED TRANSPORTED AND DISPOSED OF IN ACCORDANCE WITH THE REQUIREMENTS OF THE WORK HEALTH & SAFETY ACT 2011 AND ANY RELEVANT REQUIREMENTS OF THE WORK COVER AUTHORITY OF NSW.

#### **EARTHWORKS**

- ALL MATERIAL UNSUITABLE FOR USE ON SITE SHALL BE DISPOSED OF OFF SITE TO AN APPROVED DISPOSAL SITE.
- 2. EARTHWORKS TO BE CARRIED OUT TO THE SATISFACTION OF THE REMEDIATION ACTION PLAN. ALL TESTING OF EARTHWORKS SHALL BE UNDERTAKEN AS NOMINATED IN THE SPECIFICATION
- 3. AT THE TOP OF ALL EXCAVATED BATTERS OVER 1.0m HIGH PROVIDE A BARRIER FENCE.
- 4. ALL EXCAVATION WORKS WITHIN TREE PROTECTION ZONES ARE TO BE HAND DUG ONLY. PROJECT ARBORIST TO CONFIRM AND APPROVE METHODOLOGY PRIOR TO CONSTRUCTION COMMENCEMENT.
- 5. THE CONTRACTOR IS RESPONSIBLE FOR ALL TEMPORARY WORKS INCLUDING SHORING AND BATTERS. ALL TEMPORARY WORKS ARE TO BE DESIGNED IN ACCORDANCE WITH AUSTRALIAN STANDARDS AND CODES OF PRACTICE BY A SUITABLY QUALIFIED ENGINEER
- 6. THE CONTRACTOR IS RESPONSIBLE FOR MANAGING SITE RISKS ASSOCIATED WITH FALLS FROM HEIGHT INCLUDING INSTALLING SAFETY BARRIERS AROUND EXCAVATIONS WHERE APPROPRIATE
- 7. EXCAVATED MATERIAL MAY BE USED AS STRUCTURAL FILL PROVIDED IT COMPLIES WITH THE SPECIFICATION FOR FILL MATERIAL AND THE PLACEMENT MOISTURE CONTENT COMPLIES WITH GEOTECHNICAL CONSULTANTS REQUIREMENTS. AND ALLOWS FILLING TO BE PLACED AND PROOFROLLED IN ACCORDANCE WITH RMS R44 SPECIFICATION
- 8. FILLING IS TO BE UNDERTAKEN IN MAX 200mm LOOSE LAYERS AND 150mm LOOSE LAYERS AT DEPTHS <500mm BELOW PROPOSED BULK EARTHWORKS LEVELS.
- 9. SELECTED MATERIAL FOR USE AS GENERAL FILL SHALL BE UNIFORM IN CLASSIFICATION, WELL GRADED, COARSE, GRANULAR AND FREE DRAINING, MAX PARTICLE SIZE 75mm, COMPACTED IN 150mm MAX LAYERS TO OBTAIN 100% SMDD AS DETERMINED BY TEST AS 1289.5.1.1.
- 10. EXCAVATED SURFACES ARE TO BE CHECKED WITH A 10 TONNE ROLLER (MIN) FOR SOFT OR COMPRESSIBLE ZONES AREAS WITH MORE THAN 3 mm MOVEMENT UNDER ROLLER. SUCH ZONES ARE TO BE OVER EXCAVATED TO A MIN DEPTH OF 300 mm AND REPLACED WITH COMPACTED SELECTED MATERIAL IN ACCORDANCE WITH NOTE 8.

#### **EXISTING SERVICES**

- 1. THE CONTRACTOR SHALL UNDERTAKE POTHOLING AND/OR INVESTIGATION WORKS TO LOCATE ALL EXISTING SERVICES PRIOR TO COMMENCING WORKS.
- 2. EXISTING SERVICES INFORMATION SHOWN ON THESE PLANS ARE LOCATED FROM INFORMATION SUPPLIED BY THE SURVEYOR AND ARE NOT GUARANTEED COMPLETE OR CORRECT. ALL SERVICE LOCATIONS ARE TO BE VERIFIED BY THE CONTRACTOR PRIOR TO ANY EXCAVATION OR CONSTRUCTION WORKS. ARUP ACCEPTS NO RESPONSIBILITY FOR THE ACCURACY OF THIS INFORMATION.
- 3. ALL REMOVED. DIVERTED AND PROTECTED UTILITIES WORKS RELATED TO EXISTING SERVICES NEED TO BE VERIFIED AND APPROVED BY HEALTH INFRASTRUCTURE AND RELEVANT SERVICE AUTHORITIES.
- 4. ALL ACCESSIBLE EXISTING SERVICE PITS AND VALVES TO BE RETAINED MUST HAVE THEIR COVERS RAISED OR LOWERED TO THE PROPOSED SURFACE LEVELS TO MAINTAIN ACCESS. SERVICE PITS AND COVERS MAY NEED REPLACEMENT FOR CHANGED LOADING CONDITIONS.
- 5. IMPACTS ON EXISTING UTILITIES. INCLUDING REQUIRED DIVERSIONS. PROTECTION AND MODIFICATION WORKS ARE NOT SHOWN IN THIS SET OF CIVIL ENGINEERING DRAWINGS. REFER TO DOCUMENTATION OF THE RELEVANT ENGINEERING AND TECHNICAL SERVICE CONSULTANTS. UTILITIES WORKS ARE TO BE COORDINATED WITH CIVIL ENGINEERING WORKS.

#### **ASBESTOS**

- 1. CONTAMINATED MATERIALS ARE KNOWN TO EXIST ON SITE, REFER TO CONTAMINATION REPORT.
- 2. REFER TO THE REMEDIATION ACTION PLAN FOR THIS SITE. THE CONTRACTOR IS TO ATTEND TO ALL ISSUES IN THE REMEDIATION ACTION PLAN BEFORE COMMENCING ANY DEMOLITION OR CONSTRUCTION WORKS.
- 3. ALL AIR MONITORING EQUIPMENT TO BE IN PLACE PRIOR TO COMMENCING ANY DEMOLITION OR CONSTRUCTION WORKS,

#### **ROADWORKS GENERAL**

- 1. THE CONTRACTOR SHALL OBTAIN ALL LEVELS FROM ESTABLISHED BENCH MARKS ONLY AS SUPPLIED BY THE APPOINTED SURVEYORS.
- 2. NO WORK TO BE CARRIED OUT ON ADJOINING PROPERTIES WITHOUT THE WRITTEN PERMISSION FROM THE OWNER.
- 3. VEHICULAR ACCESS AND ALL SERVICES ARE TO BE MAINTAINED AT ALL TIMES TO AREAS AFFECTED BY CONSTRUCTION
- 4. ALL RUBBISH, BUILDINGS, SHEDS, FENCES, AND POWER POLES ARE TO BE REMOVED IN ACCORDANCE WITH COPC AND RMS SPECIFICATIONS.
- 5. CONTRACTOR TO INSTALL ROOT CONTROL BARRIERS ALONG LENGTH OF KERBS AND RETAINING WALLS ADJACENT TO LANDSCAPED ARFAS.

#### **GEOTECHNICAL INSPECTION & TESTING**

- 1. IT IS INCUMBENT ON THE CONTRACTOR TO ENSURE ALL EARTHWORKS ARE UNDERTAKEN UNDER LEVEL 2 GEOTECHNICAL INSPECTION AND TESTING SUPERVISION AS DEFINED IN AS 3798-2007. ALL COSTS SHALL BE BORNE BY THE CONTRACTOR.
- 2. THE LEVEL 2 GEOTECHNICAL AND TESTING AUTHORITY (GITA) SHALL PROVIDE A REPORT DETAILING ALL THE INSPECTIONS. SAMPLING AND TESTING IT HAS CARRIED OUT TOGETHER WITH PLAN LOCATIONS AND A SUMMARY OF RESULTS.
- 3. THE GITA IS ALSO REQUIRED TO EXPRESS AN OPINION ON THE COMPLIANCE OF THE SITE WORKS WITH THE SPECIFICATION AND DRAWINGS BY WAY OF EVIDENCE TO THE ENGINEER FOR **CERTIFICATION PURPOSES**

#### **DRAINAGE**

- 1. PRIOR TO THE WORKS COMMENCING AND FOLLOWING COMPLETION OF WORKS, A CCTV INSPECTION AND REPORT IS TO BE UNDERTAKEN FOR ALL STORMWATER PIPES AND CULVERTS TO BE RETAINED AND AFFECTED BY THE PROPOSED WORKS.
- 2. ALL EXISTING REDUNDANT PITS AND PIPES WHICH ARE NO LONGER REQUIRED ARE TO BE GROUT FILLED UNLESS NOTED OTHERWISE.
- 3. ALL DRAINAGE TRENCHES WHERE PITS AND PIPES ARE REMOVED ARE TO BE BACKFILLED AND COMPACTED WITH CLEAN MATERIAL WHICH COMPLIES WITH THE COPC SPECIFICATIONS AND IN ACCORDANCE TO THE GEOTECHNICAL ENGINEERS RECOMMENDATIONS.
- 4. ALL PITS OVER 1.2 m IN DEPTH TO BE PROVIDED WITH STEP IRONS IN **ACCORDANCE WITH AS 4198.**
- 5. PIT AND PIPE CONCRETE TO HAVE A MINIMUM COMPRESSIVE STRENGTH OF 32 MPa AT 28 DAYS U.N.O.
- 6. THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND CONSTRUCTION OF ALL TEMPORARY DRAINAGE NECESSARY TO DRAIN THE SITE DURING CONSTRUCTION INCLUDING TEMPORARY DIVERSIONS OF TRUNK MAINS TO ENABLE PERMANENT WORKS.
- 7. CONNECT DOWNPIPES TO THE STORMWATER SYSTEM AS EARLY AS POSSIBLE.
- 8. CONTRACTOR TO PROVIDE ROOT CONTROL BARRIERS FOR DRAINAGE PIPES WITHIN LANDSCAPED AREAS.

9. SUBSOIL DRAINAGE PIPES TO BE PROVIDED AT STORMWAT ACCORDANCE WITH COPC STANDARD DRAINAGE DRAWING

# KERB. GUTTERS. DISH DRAINS CROSSINGS

#### **EDGES**

1. THE SUPPLY, PLACING, FINISHING AND CURING OF IN-SITU FOR PAVEMENTS SHALL COMPLY WITH AS 3600. ALL CONCR Date: 3/5/2023 BE MIN N25 AT 28 DAY COMPRESSIVE STRENGTH UNO.

2. ALL KERBS, GUTTERS, DISH DRAINS AND CROSSINGS TO BE CONSTRUCTED ON 100 mm GRANULAR BASE COURSE COMP TO MINIMUM 98% MODIFIED DRY DENSITY (AS 1289 5.2.1), UNO.

- 3. CONCRETE FOOTPATHS, RAMPED AND VEHICULAR CROSSINGS SHALL BE BROOM FINISHED. UNO. ALL OTHER KERB. GUTTER AND DISH DRAINS ARE TO BE STEEL FLOAT FINISHED.
- 4. AT TIE INS WITH EXISTING KERBS. CONCRETE TO BE SAWCUT AND EXPANSION JOINTS ARE TO BE PROVIDED.
- 5. KERB EXPANSION JOINTS SHALL BE LOCATED AT 12 m CENTRES. ON TANGENT POINTS OF CURVES AND AT DRAINAGE PITS. EXPANSION JOINTS ARE TO BE FORMED FROM 10 mm COMPRESSIBLE CORK FILLER BOARD FOR THE FULL DEPTH OF THE SECTION AND CUT TO
- 6. FOR INTEGRAL KERBS THE EXPANSION JOINTS MUST BE 3 mm WIDE AND LOCATED AT 3 m CENTRES AND AT LOCATIONS NOTED ABOVE
- 7. WEAKENED PLANE JOINTS TO BE MIN 3 mm WIDE AND LOCATED AT 3 m CENTRES EXCEPT FOR INTEGRAL KERBS WHERE WEAKENED PLANE JOINTS AREA TO MATCH THE JOINT LOCATIONS IN SLABS
- 8. SAWCUTTING MUST PROCEED WITHIN 24 HOURS OF PLACING CONCRETE.
- 9. SUBSOIL DRAINS ARE TO BE PROVIDED BEHIND ALL KERBS. FLUSHING POINTS ARE TO BE PROVIDED AT MAXIMUM 30 m SPACING AND AT END OF PIPES.
- 10. WHEEL STOPS ARE TO BE PRECAST CONCRETE UNITS WITH PREDRILLED HOLES LOCATED 300 mm FROM EACH END FOR FIXING TO GROUND SURFACE, SIZE: 2000 mm x 150 mm x 100 mm HIGH, DRIVE 12 mm DIAMETER GALVANISED STEEL RODS 600 mm INTO THE GROUND AND TO FINISH 25 mm BELOW THE TOP OF THE WHEEL STOP.
- 11. EXISTING KERBS ARE TO BE COMPLETELY REMOVED WHERE NEW KERBS ARE SHOWN. REPLACEMENT OF EXISTING KERB AND GUTTER SHALL INCLUDE:
  - a. SAW CUTTING OF EXISTING ROAD PAVEMENT 600 mm
  - MINIMUM FROM THE LIP OF GUTTER OR EDGE OF SEAL: RECONSTRUCT EXISTING ALLOTMENT DRAINAGE PIPES INTO THE NEW KERB WITH A 100 mm DIA PIPE OR TO LOCAL APPROVAL AUTHORITY STANDARD:
  - c. RECONSTRUCTION OF REMOVED ROAD SURFACE (600 mm MINIMUM) WITH NEW BASECOURSE AND SURFACE TO MATCH EXISTING THICKNESSES.

#### FOOTPATH AND VEHICULAR ROADWAY CONCRETE

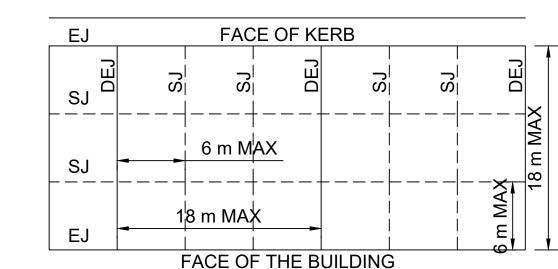
#### FINISH

1. ALL EXPOSED CONCRETE PAVEMENTS ARE TO BE BROOM FINISHED AT RIGHT ANGLES TO THE ALIGNMENT OF THE PAVEMENT TO PROVIDE A UNIFORM SLIP-RESISTANT SURFACE

- 2. ALL EDGES OF THE CONCRETE PAVEMENT INCLUDING KEYED AND DOWELED JOINTS ARE TO BE FINISHED WITH AN EDGING TOOL.
- 3. CONCRETE PAVEMENTS WITH GRADES GREATER THAN 10% SHALL BE HEAVILY BROOMED FINISHED.

#### PAVEMENT (VEHICULAR) JOINTING

- 1. DOWELED EXPANSION JOINTS (DEJ) OR KEYED JOINTS SHALL BE LOCATED AT MAX 18 m CENTRES
- 2. SAWN JOINTS (SJ) SHALL BE LOCATED AT MAX 6 m CENTRES OR 1.5 x THE WIDTH OF THE PAVEMENT.
- 3. PROVIDE 10 mm WIDE FULL DEPTH EXPANSION JOINTS (EJ) BETWEEN BUILDINGS AND ALL CONCRETE, UNO.
- 4. THE TIMING OF THE SAW CUT IS TO BE DETERMINED AND CONFIRMED BY THE CONTRACTOR ON SITE, SITE CONDITIONS WILL DETERMINE HOW MANY HOURS AFTER THE CONCRETE POUR BEFORE THE SAW CUTS ARE COMMENCED.
- 5. GENERALLY THE VEHICULAR PAVEMENT JOINTS SHOULD FOLLOW THE BELOW LAYOUT, UNLESS SHOWN OTHERWISE ON THE CIVIL DRAWINGS.



Health NSW Infrastructure

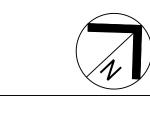
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**REF Approval No:** 10/2023

| Signed by:

2030g

 2 21/02/23 DESIGN DEVELOPMENT
 1 25/11/22 DESIGN DEVELOPMENT KEY PLAN



CONSULTANTS



CLIENT







PROJECT **WESTMEAD** INTEGRATED MENTAL **HEALTH COMPLEX (IMHC) EARLY WORKS CIVIL WORKS** 

PROJECT NO. 281916-00

**DRAWING TITLE GENERAL NOTES** SHEET 1 OF 2

**DESIGN DEVELOPMENT** 

SCALE @ B1 NOT TO SCALE

CPL

CHECKED

DRAWING NO. IMHC-CV-DG-2006

DT

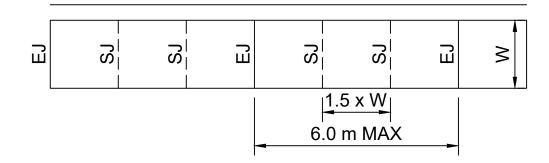
APPROVED

**REVISION** 

**NOT FOR CONSTRUCTION** 

#### FOOTPATH JOINTING

- 1. EXPANSION JOINTS (EJ) ARE TO BE LOCATED WHERE POSSIBLE AT TANGENT POINTS OF CURVES AND ELSEWHERE AT A MAXIMUM SPACING OF 6 m.
- 2. SAWN JOINTS (SJ) ARE TO BE LOCATED AT 1.5 x WIDTH OF THE PAVEMENT.
- 3. WHERE POSSIBLE THE JOINTS SHALL BE LOCATED TO MATCH THE KERBING AND OR ADJACENT PAVEMENT JOINTS.
- 4. GENERALLY THE FOOTPATH PAVEMENT JOINTS SHOULD FOLLOW THE BELOW LAYOUT, UNLESS SHOWN OTHERWISE ON THE CIVIL DRAWINGS.



#### LINEMARKING

1. ALL LINE MARKING TO BE IN ACCORANCE WITH ROADS AND MARITIME SERVICES QA SPEC R141 AND ROADS AND MARITIME SERVICES DELINEATION GUIDELINES.



DETERMINED – APPROVAL

20309

**REF Approval No:** 10/2023 **Date:** 3/5/2023

Signed by:

2 21/02/23 DESIGN DEVELOPMENT
1 25/11/22 DESIGN DEVELOPMENT
0 30/09/22 DESIGN DEVELOPMENT
REV DATE KEY PLAN

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CLIENT







PROJECT **WESTMEAD** INTEGRATED MENTAL HEALTH COMPLEX (IMHC) EARLY WORKS
CIVIL WORKS

PROJECT NO. 281916-00

DRAWING TITLE GENERAL NOTES SHEET 2 OF 2

**DESIGN DEVELOPMENT** 

CPL DT APPROVED **AC** CHECKED

SCALE @ B1 NOT TO SCALE

DRAWING NO.

**NOT FOR CONSTRUCTION** 

REVISION IMHC-CV-DG-2007

ING FEA	<u>TURES</u>	EXISTING UTILITIES	
	BOUNDARY	—— Е —— Е ——	ELECTRICAL
	DRIVEWAY, PATH OR CYCLEWAY	——E(D) ——E(D) ——	ELECTRICAL - DETECTED
	TOP OF BATTER	LV LV	ELECTRICAL - LOW
	BOTTOM OF BATTER	——— HV ——— HV ———	ELECTRICAL - HIGH
ND GUTTER	KERB AND GUTTER	——————————————————————————————————————	ELECTRICAL - DBYD
B ONLY	KERB ONLY	——— S ——— S ———	SEWER
-	KERB INLET PIT	——————————————————————————————————————	SEWER - DETECTED
DRAIN D	DISHED CROSSING	—— IS——— IS———	SEWER - TUNNEL
· · · · · · · · · · · · · · · · · · ·	FENCE	—— D —— D ——	STORMWATER
P · GP · GP · GP ·	EDGE OF GARDEN	SW SW	STORMWATER
+ 100.	SPOT LEVEL	——SW(D) ———SW(D) ——	STORMWATER - DETECTED
0	BOLLARDS	—— G —— G ——	GAS
17.0	CONTOURS	G(D)G(D)	GAS - DETECTED
	TREE	——————————————————————————————————————	COMMS - DBYD
		——————————————————————————————————————	COMMS - DETECTED
<b>A</b>	BENCH MARK	W(D)W(D)	WATER - DETECTED
∎ TEL	TELSTRA PIT	—— TW——— TW——	TRADE WATER
■ COM	COMMS PIT	—— F —— F ——	
) ELP	ELECTRIC LIGHT POLE		
I EPIT	ELECTRICITY PIT		STING UTILITIES, INCLUDING REQUIRED ECTION AND MODIFICATION WORKS ARE
■SWB	ELECTRICITY SWITCHBOARD	NOT SHOWN IN THIS SET OF CIVIL ENGINEERING DRAWINGS.	
EPLQ	ELECTRICAL PLAQUE	REFER TO DOCUMENTATION OF THE RELEVANT ENGINEERING AND TECHNICAL SERVICE CONSULTANTS. UTILITIES WORKS	
EROD	EARTHING ROD		POPOSED STORMWATER IS COVERED IN
EL	ELECTRICITY BOX	** EXISTING AND PROPOSED STORMWATER IS COVERED IN THIS CIVIL DESIGN PACKAGE	
SWIPE	CARD SWIPE	SEDIMENT AND EROSION	
] AC	AIR CONDITIONER UNIT	SEDIMENT AN	ND LNOSION
POLE	POLE		TEMPORARY CONSTRUCTION EXIT
TM	TICKET MACHINE		SEDIMENT FENCE
VM	VENDING MACHINE		SEDIMENT TRAP
SS	STREET SIGN		SEDIMENT TRAP
BOL	BOLLARD		FABRIC STOCKING KERB INLET
CLID	PIT WITH CONCRETE LID		DIVERSION BANK AND CHANNEL
MLID	PIT WITH METAL LID		STOCKPILE
GIP	GRATED INLET PIT		
•KIP	KERB INLET PIT		OVERLAND FLOW
SWLID	STORMWATER PIT		SEDIMENT BASIN
MH	MANHOLE	CIVIL WORKS	
] IC	INTERCOM	OIVIL WORKS	EXTENT OF WORKS BOUNDARY
⊐ BG	BOOM GATE		
	GATE	K&G	EDGE OF BITUMEN
SIP	SEWER INSPECTION POINT	KO	KERB AND GUTTER TO PCC STD DRG No DS1
SEV	SEWER VENT	T1	KERB ONLY TO PCC STD DRG No DS1
MW	MONITOR WELL	••••••••	TURNING LINE
SMH	SEWER MANHOLE	<u>C1</u>	CONTINUITY LINE
LH	LAMP HOLE	BB	DIVIDING BARRIER LINE
SV	STOP VALVE		MINOR CONTOUR
HYD	HYDRANT	17.0	MAJOR CONTOUR
FHYD	FIRE HYDRANT	. X . X	DIGUES / DEMOVE
FHR	FIRE HOSE REEL	-\-\-\-\-\-\-\-\-	FENOE
WM	WATER METER		- <del></del>
TAP	WATER TAP	PROPOSED D	RAINAGE
ICV	IRRIGATION CONTROL VALVE		STORMWATER PIPE
GAS	GAS PLACUE	Ø375	PIPE SIZE AND FLOW ARROW
PLQ	GAS MARKER		STORMWATER GRATED INLET PIT
GMKR	GAS PILLAR		INSPECTION PIT
GPIL NVL	GAS PILLAR NITRO VALVE		KERB INLET PIT
NVL	INSPECTION OPENING		TRENCH DRAIN
IP	INSPECTION OF LINING  INSPECTION PIT	01-1	PIT LABEL LINE NUMBER 01 / PIT NUMBER 1
	DIP POINT	—— D —— D ——	EXISTING STORMWATER
		CUL	EXISTING STORMWATER
DIP	VEHICLE CROSSING	—— SW —— SW ——	LAISTING STORWWATER
(VC)	VEHICLE CROSSING PRAM CROSSING	—— SW —— SW ———————————————————————————	
DIP (VC)			

DEPTH TO SERVICE

UNABLE TO OPEN PIT

END OF TRACE OF SERVICE

QUALITY LEVEL 'D' TRACE

(D0.8)

UTO

QLD

RETAINING WALL

RW2

TYPE 2 - > 1 m

CITY OF PARRAMATTA COUNCIL (COPC)
STANDARD DRAWINGS TO BE REFERRED TO AS
NOTED ON PLANS AND SPECIFICATION

DRAWING No. TITLE

DS1 KERBS AND LAYBACKS

DS2 ROOFWATER OUTLET

DS3 FOOTPATH

DS4 KERB RAMP

DS5 DISH DRAIN CROSSING

DS5 DISH DRAIN CROSSING DETAIL

DS6 TYPICAL CYCLEWAY/SHARED PATHWAY

DS8 STANDARD VEHICULAR CROSSING

DS9 HEAVY DUTY VEHICULAR CROSSING

VEHICULAR CROSSING PROFILES

DS11B SIGN SUPPORT POST

DS21 STANDARD KERB INLET PIT (ON GRADE)

DS23 STANDARD SURCHARGE PIT
DS24 GRATED SAG PIT USING PRECAST LINTEL
DS25 SURFACE INLET PIT FOR FOOTPATHS
DS25 SURFACE INLET PIT FOR OPEN SPACE AREAS

DS26 STANDARD JUNCTION PIT
DS26 HEAVY DUTY JUNCTION PIT

DS31 TWIN GRATED GULLY PIT WITH EXTENDED KERB INLET

DS33 SUBSOIL DRAINAGE DETAILS

DS36 HEADWALLS FOR CULVERTS UP TO 900 DIAMETER
DS37 STANDARD FOR INSTALLATION OF PIPELINES AND

RESTORATION OF TRENCHES

DS43 TREE PROTECTION TIMBER HOARDING

# TRANSPORT FOR NEW SOUTH WALES (TfNSW) STANDARD DRAWINGS TO BE REFERRED TO AS NOTED ON PLANS AND SPECIFICATION

DRAWING REF No. TITLE

DS2012/000293 PAVEMENT STANDARD DRAWINGS RIGID PAVEMENT

BICYCLE PATH DESIGN

DS2014/005559 PAVEMENT STANDARD DRAWINGS RIGID PAVEMENT VOLUME CJ - JOINTED REINFORCED CONCRETE PAVEMENT

NSW Health Infrastructure

#### DETERMINED – APPROVAL

REF Approval No: 10/2023

Date: 3/5/2023

Signed by:

2030g.

2 21/02/23 DESIGN DEVELOPMENT
1 25/11/22 DESIGN DEVELOPMENT
0 30/09/22 DESIGN DEVELOPMENT
REV DATE AMENDMENT

KEY PLAN

CONSULTANTS



CLIENT







PROJECT
WESTMEAD
INTEGRATED MENTAL
HEALTH COMPLEX (IMHC) EARLY WORKS
CIVIL WORKS

PROJECT NO. **281916-00** 

DRAWING TITLE

GENERAL

LEGEND

TATUS .

CHECKED

DESIGN DEVELOPMENT

DRAWN
CPL

DESIGNED
DT

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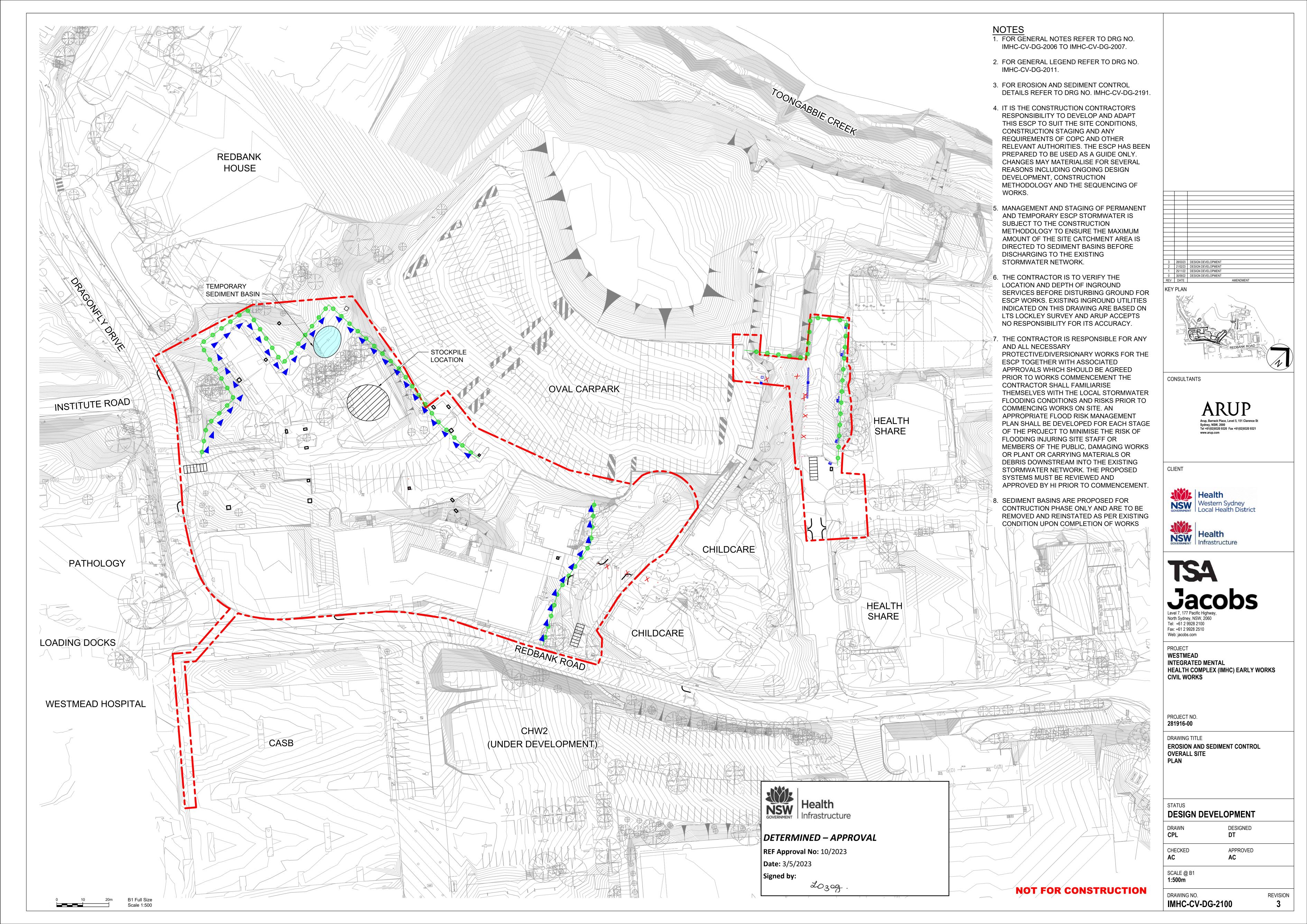
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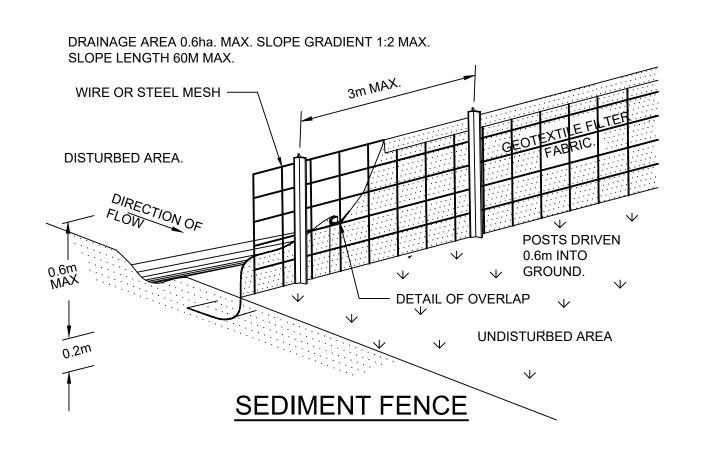
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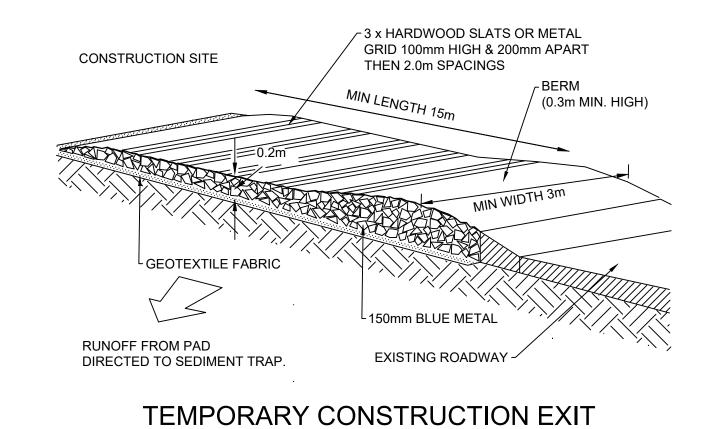
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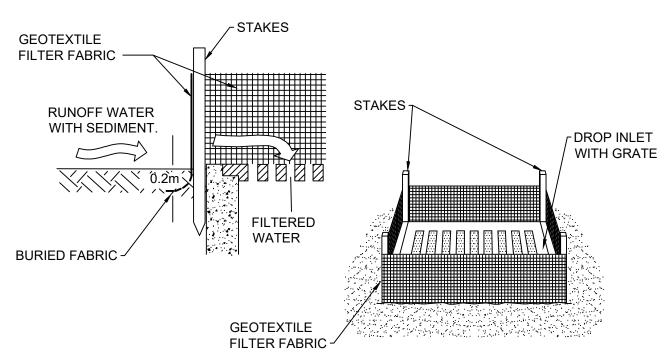
REVISION

APPROVED **AC** 

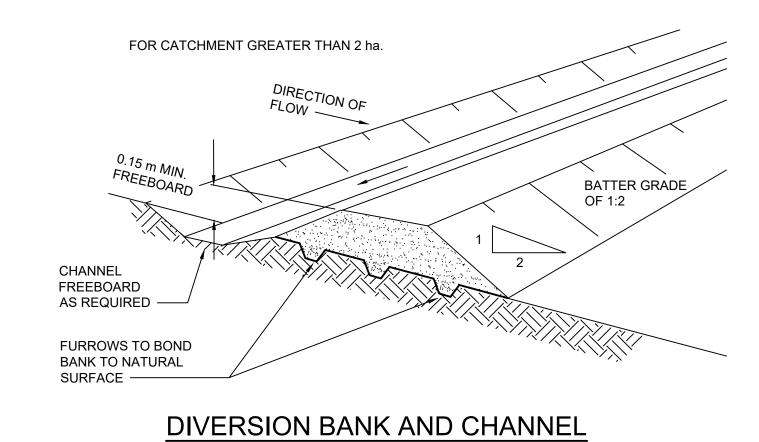


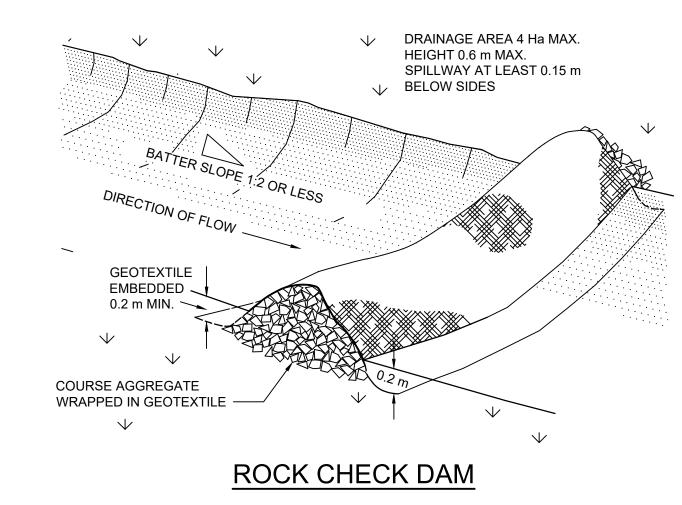


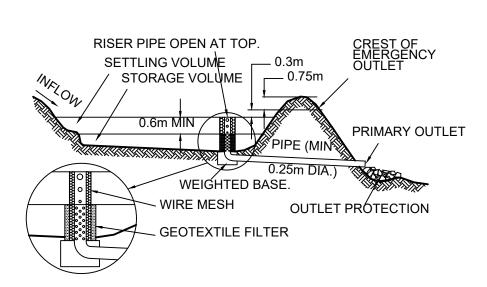




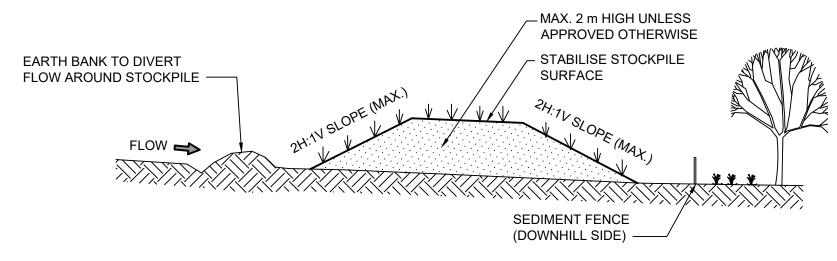




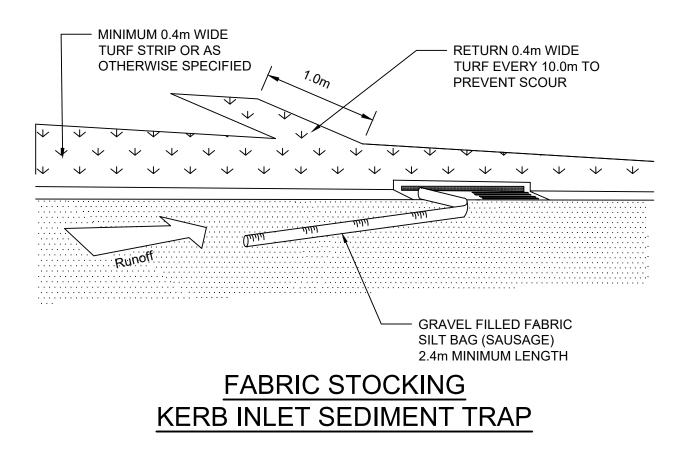




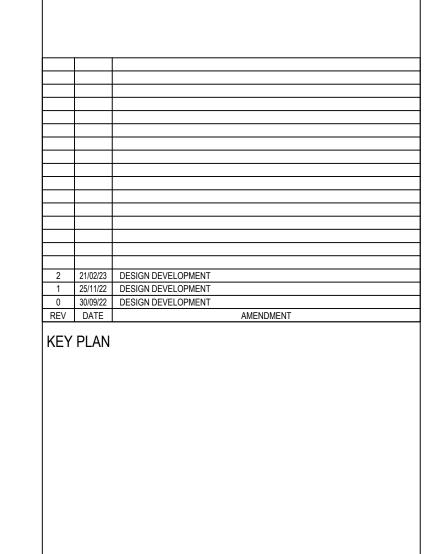




STOCKPILE







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PROJECT WESTMEAD INTEGRATED MENTAL HEALTH COMPLEX (IMHC) EARLY WORKS
CIVIL WORKS

PROJECT NO. 281916-00

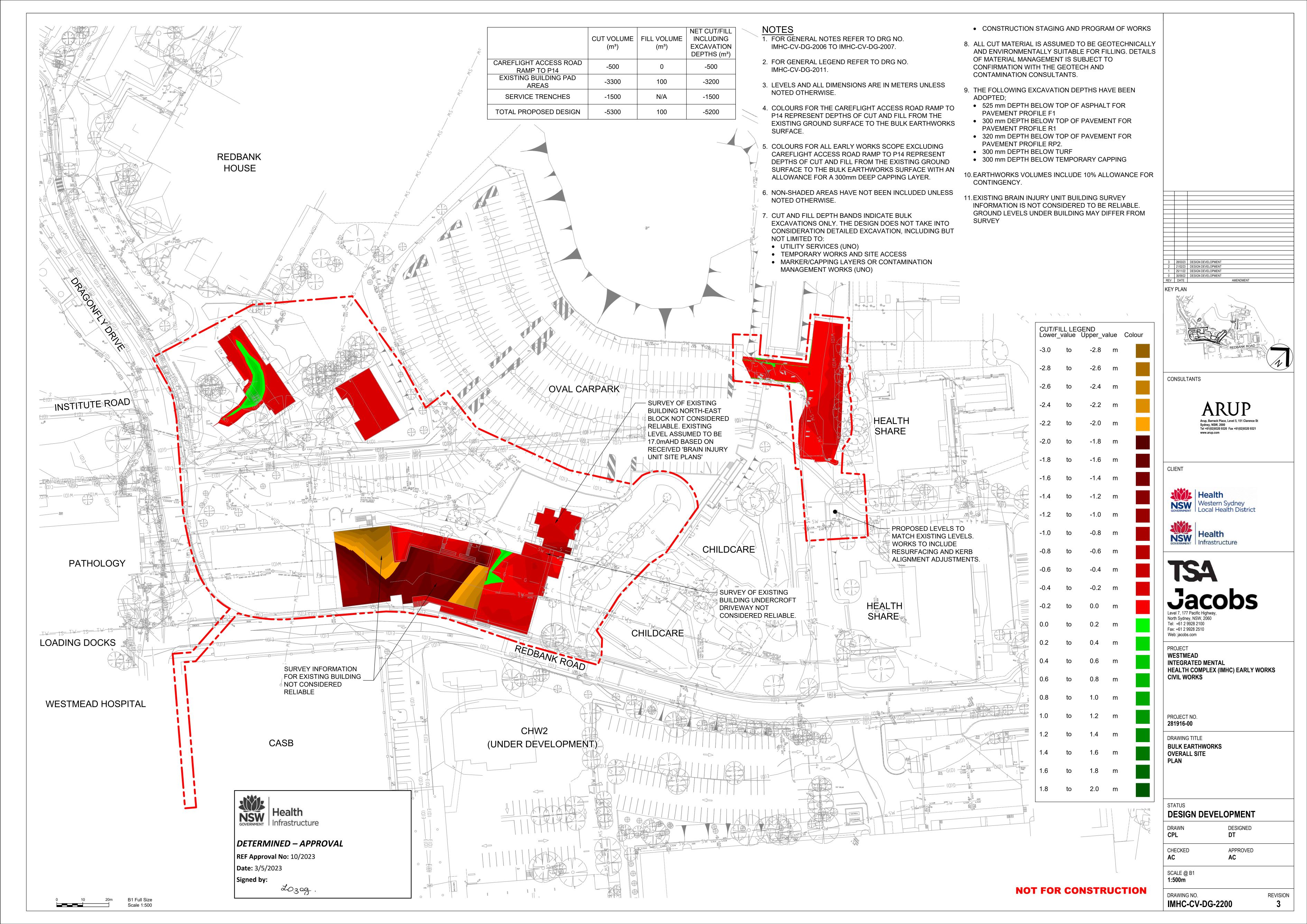
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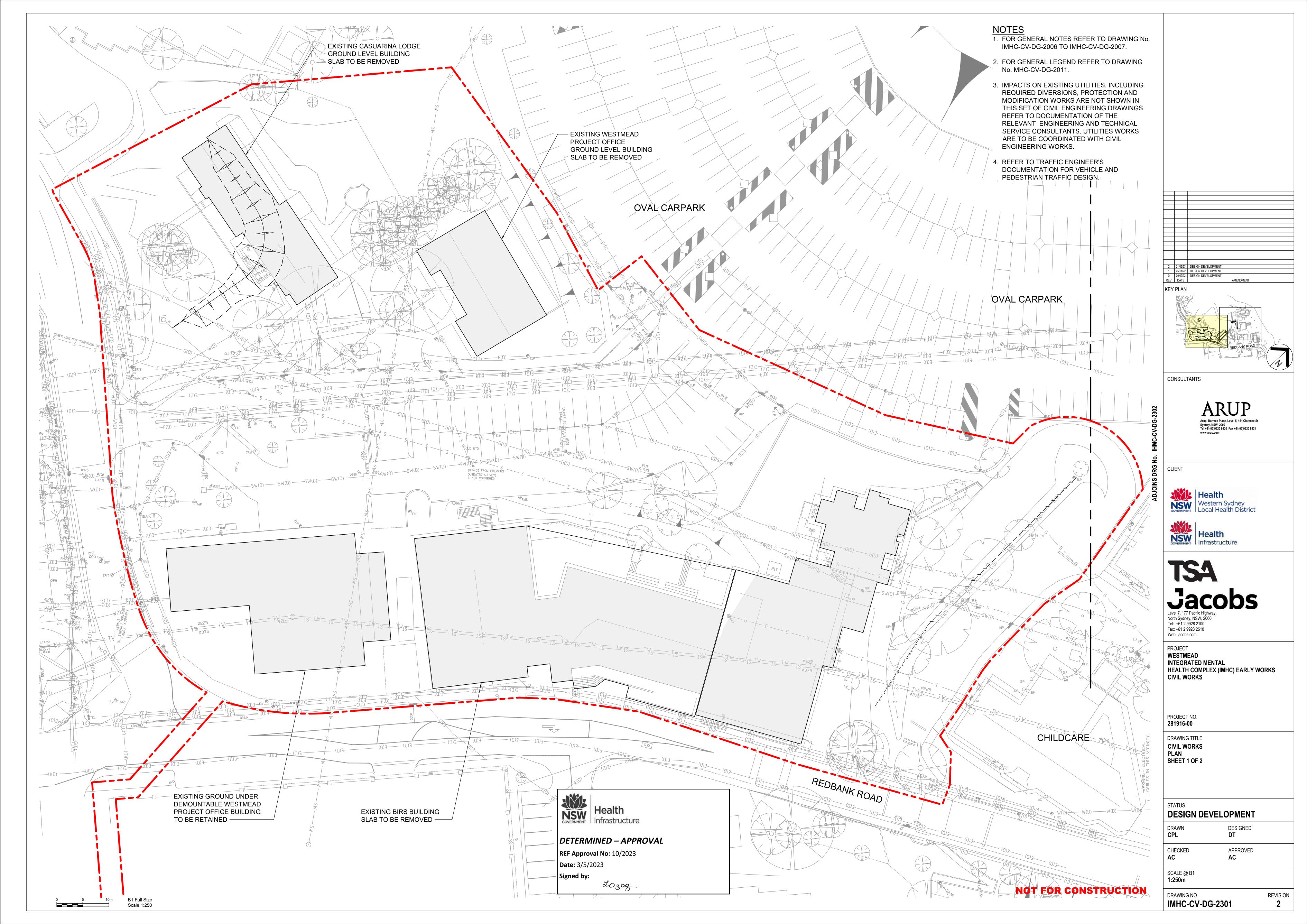
**DESIGN DEVELOPMENT** CPL DT APPROVED **AC** CHECKED

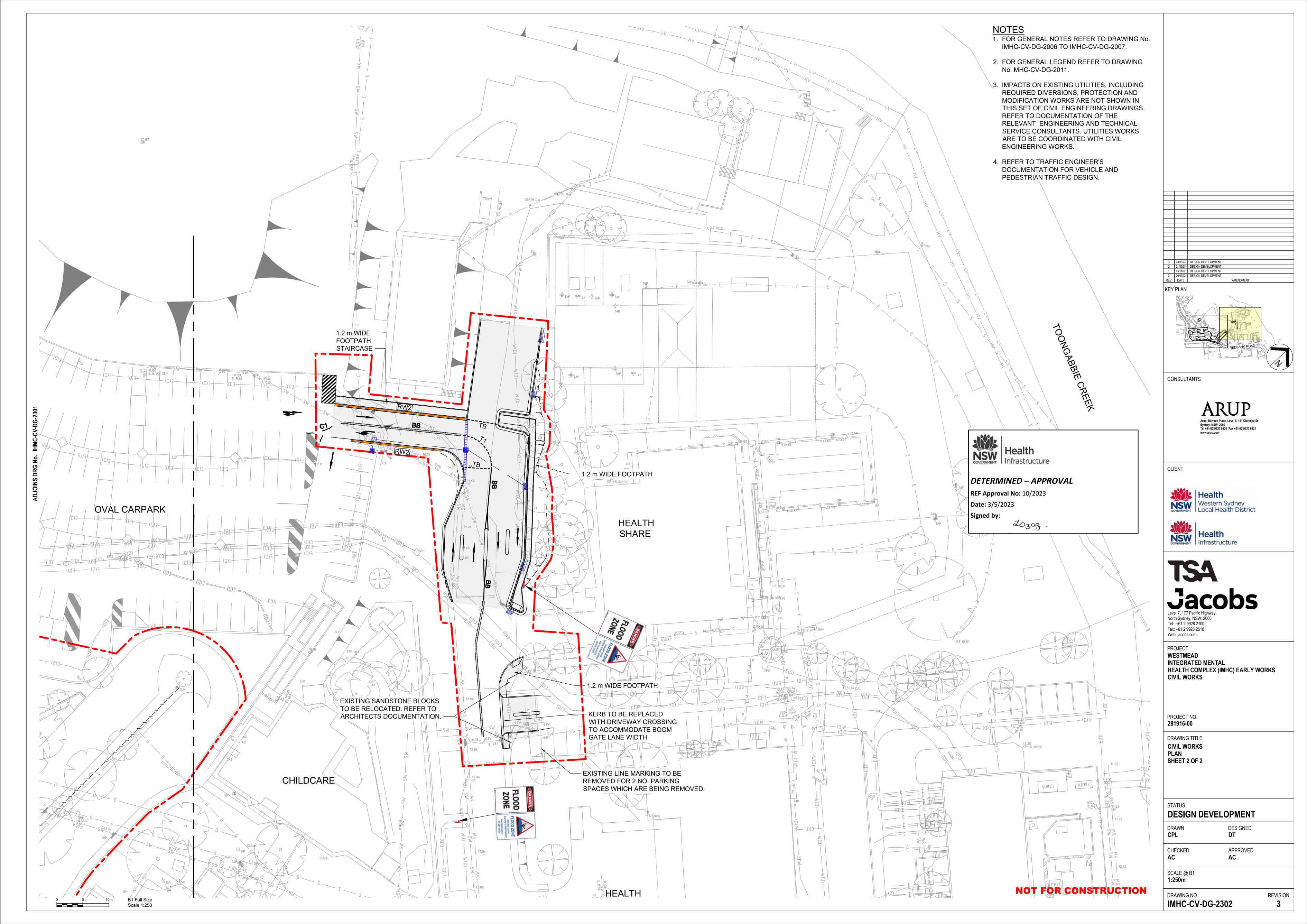
SCALE @ B1 NOT TO SCALE

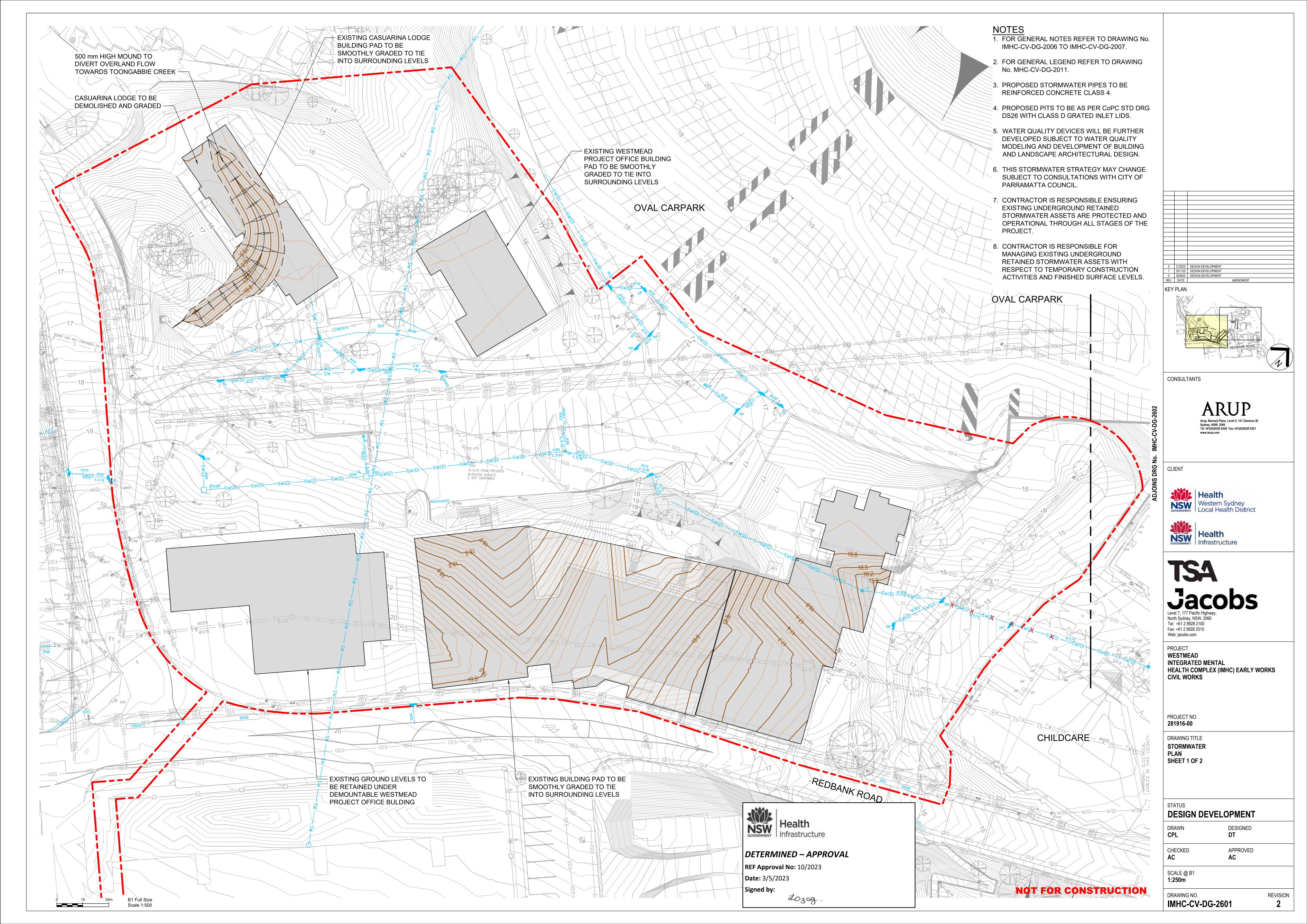
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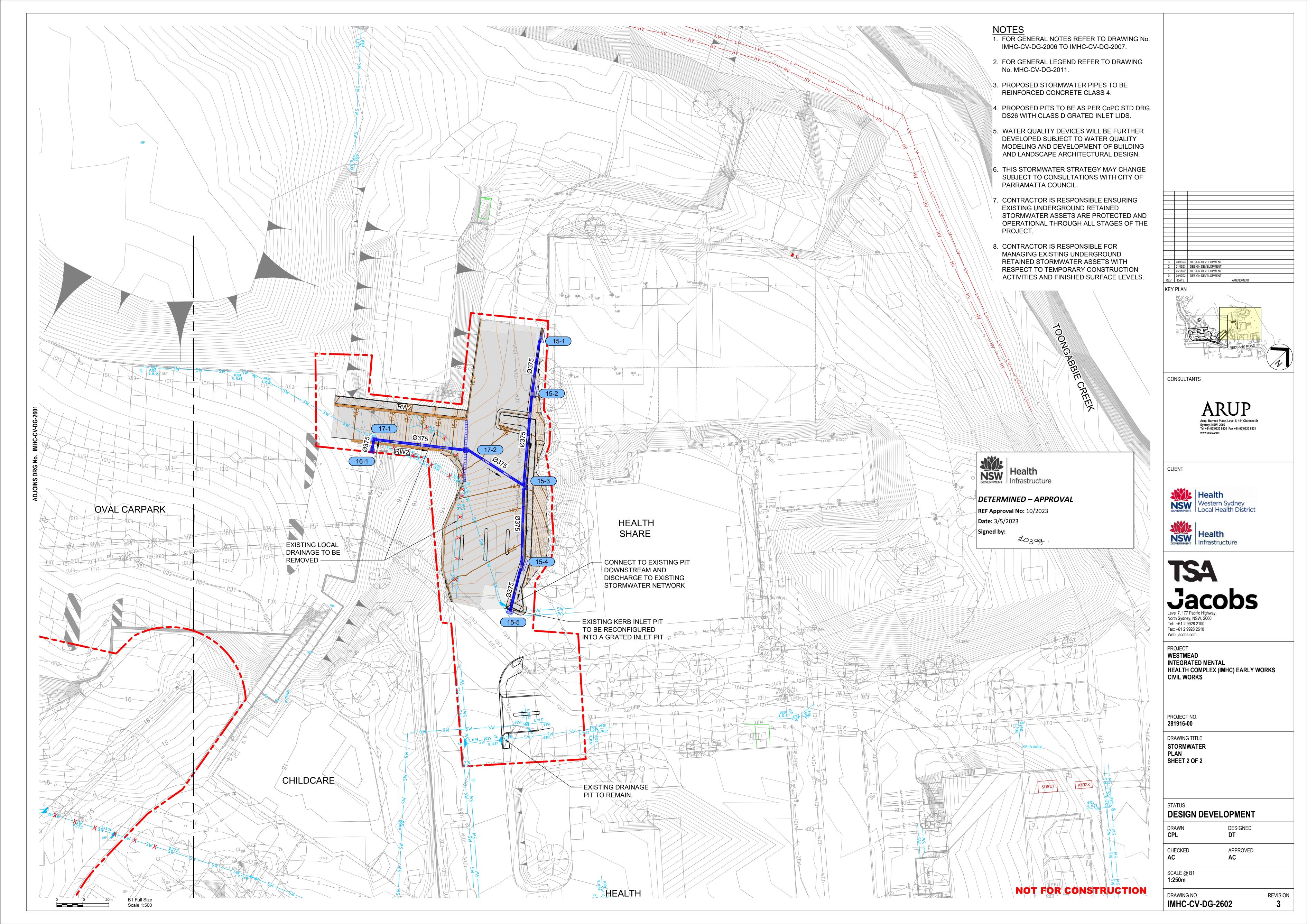
REVISION

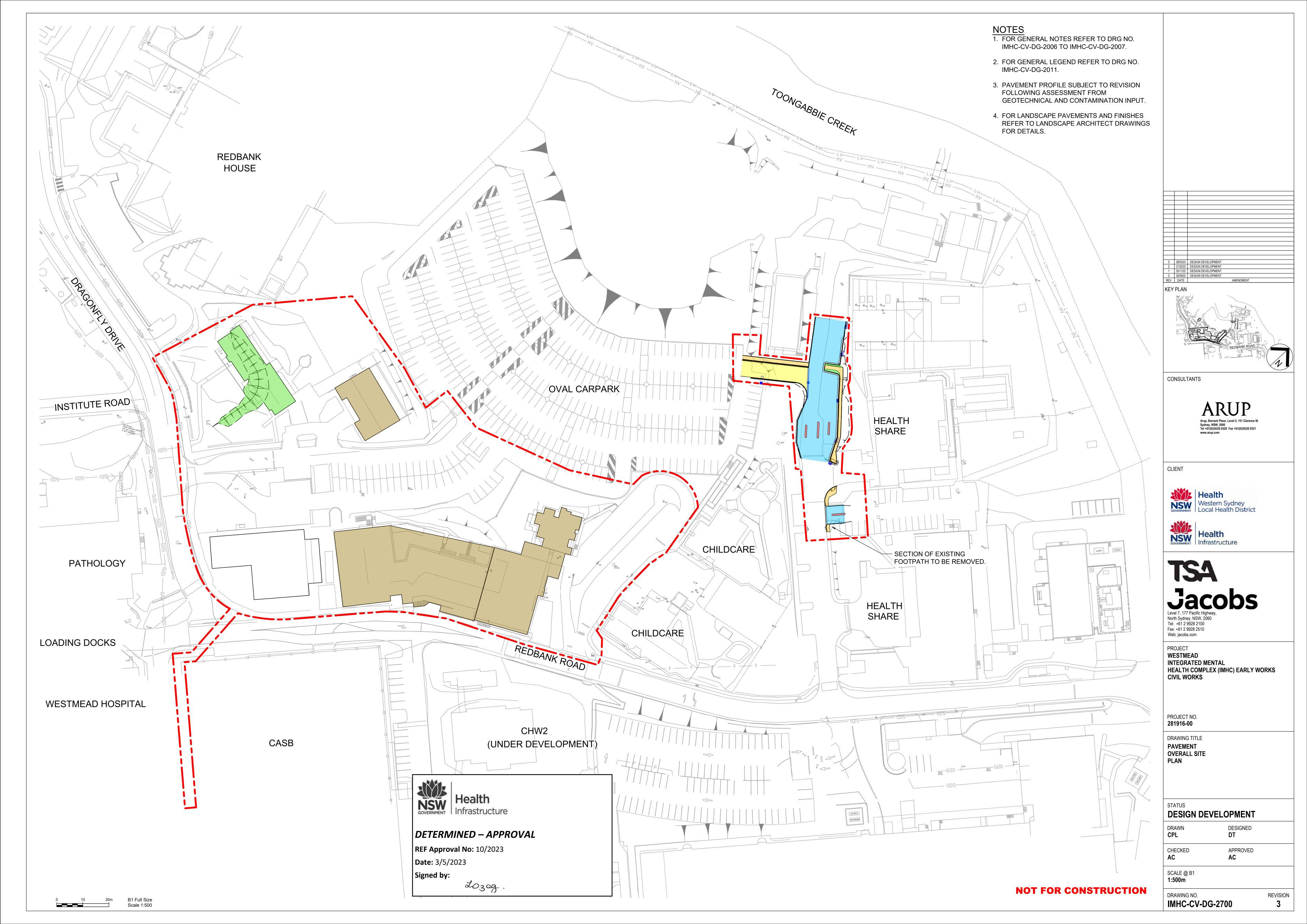


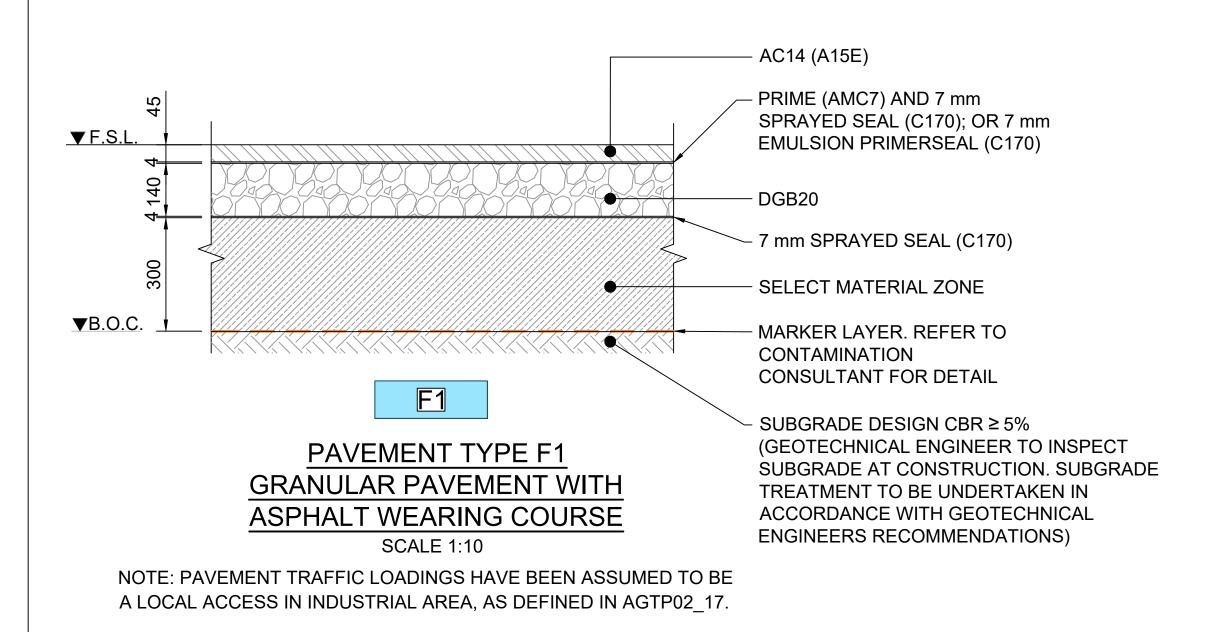


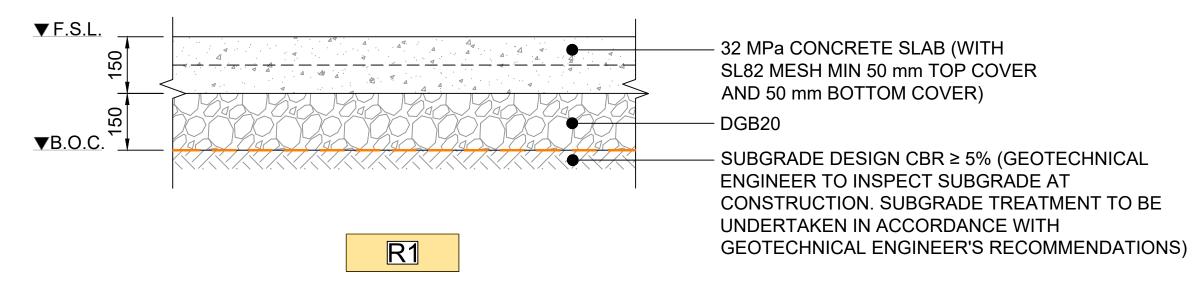




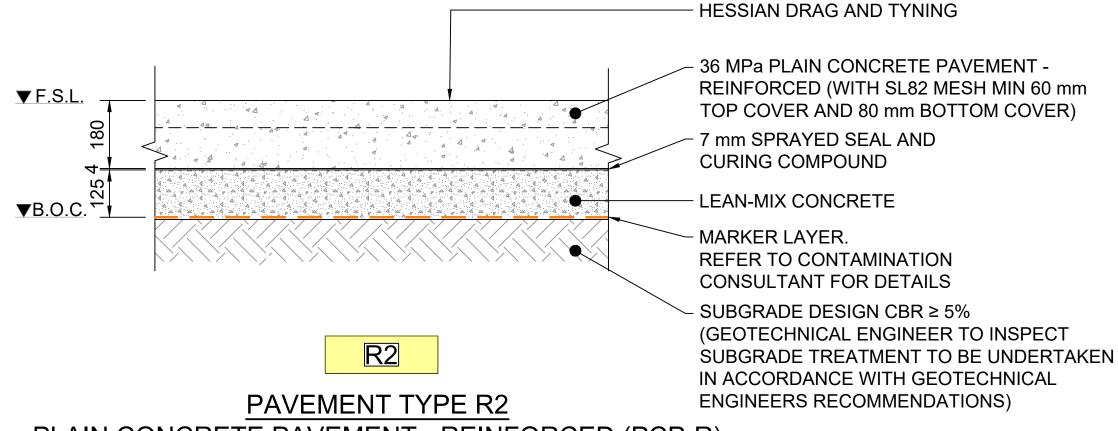








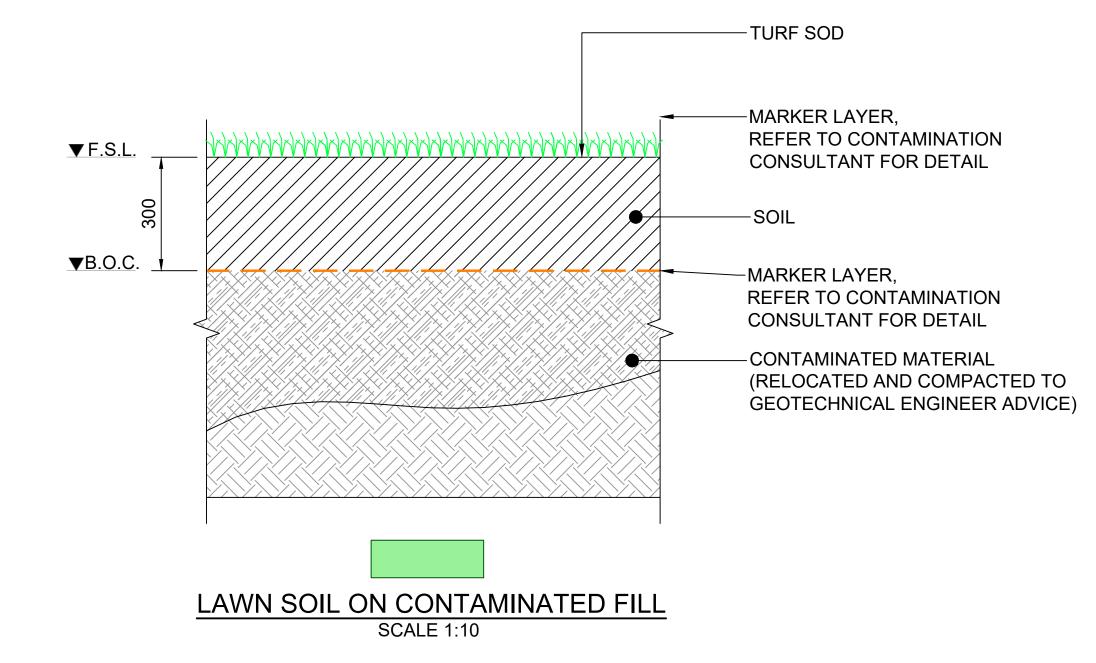
# **PAVEMENT TYPE R1** CONCRETE PEDESTRIAN FOOTPATH PAVEMENT



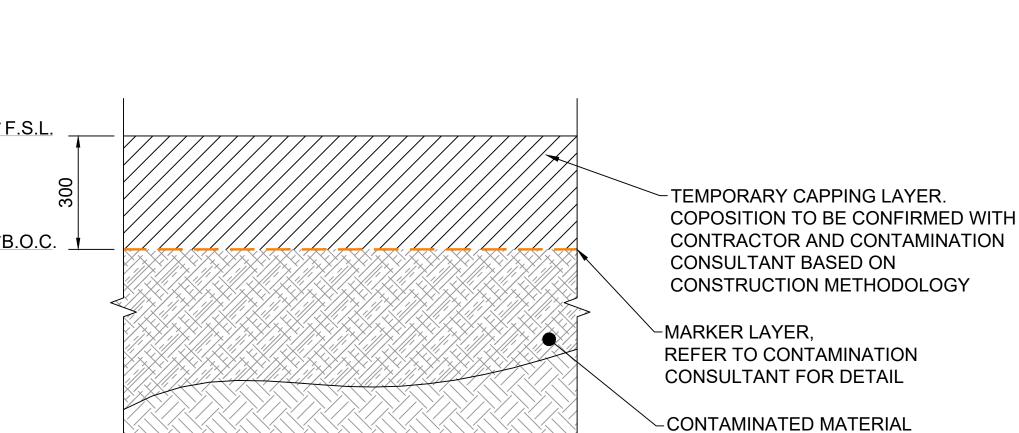
## PLAIN CONCRETE PAVEMENT - REINFORCED (PCP-R)

SCALE 1:10

NOTE: PAVEMENT TRAFFIC LOADINGS HAVE BEEN ASSUMED TO BE A LOCAL ACCESS AREA, AS DEFINED IN AGPT02 17



### ▼F.S.L TEMPORARY CAPPING LAYER **▼**B.O.C. CONTRACTOR AND CONTAMINATION CONSULTANT BASED ON **CONSTRUCTION METHODOLOGY** -MARKER LAYER, **REFER TO CONTAMINATION** CONSULTANT FOR DETAIL -CONTAMINATED MATERIAL (RELOCATED AND COMPACTED TO GEOTECHNICAL ENGINEER ADVICE)



TEMPORARY CAPPING LAYER ON CONTAMINATED FILL



**NOT FOR CONSTRUCTION** 

#### **NOTES**

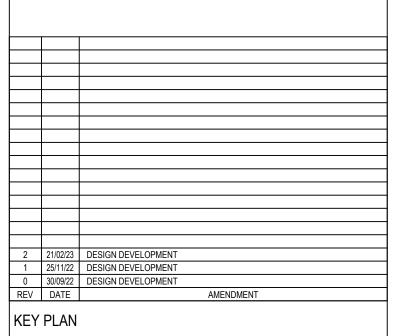
- 1. FOR GENERAL NOTES REFER TO DRG NO. IMHC-CV-DG-2006 TO IMHC-CV-DG-2007.
- 2. FOR GENERAL LEGEND REFER TO DRG NO. IMHC-CV-DG-2011.
- 3. FOOTPATH PAVEMENT JOINT DETAILS SHALL FOLLOW CoPC TYPICAL FOOTPATH DRAWING DS3. TYPICAL JOINT SPACINGS ARE AS FOLLOWS:
- EXPANSION JOINTS (EJ) EVERY 6.0 m (MAX) **INTERVALS**
- TOOLED DUMMY JOINT EVERY 1.5 m (MAX) INTERVALS
- 4. ROAD PAVEMENT JOINT DETAILS SHALL FOLLOW CoPC CBD PAVING, DRIVEWAY AND KERB RAMP DETAILS (DS40). TYPICAL JOINT SPACINGS ARE AS FOLLOWS:
- ISOLATIONS JOINTS (IJ) BETWEEN PAVEMENT AND CONCRETE KERB OR BUILDING LINE
- INTERVALS CONTROL JOINTS (CJ) - EVERY 2.0 m (MAX)

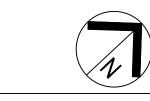
SUBSOIL DRAINAGE DETAILS (DS33).

**INTERVALS** 3. PAVEMENT SUBSOIL DRAINS SHALL FOLLOW CoPC

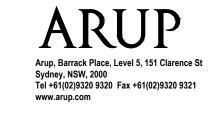
• EXPANSION JOINTS (EJ) - EVERY 6.0 m (MAX)

- 4. PAVEMENT WORKS SHALL BE UNDERTAKEN ACCORDING TO THE RECOMMENDATIONS OF THE GEOTECHNICAL INVESTIGATION.
- 5. PAVEMENT PROFILES SUBJECT TO REVISION FOLLOWING ASSESSMENT OF GEOTEHNICAL AND CONTAMINATION INPUT.





CONSULTANTS



CLIENT



NSW GOVERNMENT Infraction



Web: jacobs.com PROJECT WESTMEAD INTEGRATED MENTAL **HEALTH COMPLEX (IMHC) EARLY WORKS** 

PROJECT NO. 281916-00

**CIVIL WORKS** 

DRAWING TITLE **PAVEMENT PROFILES** 

CHECKED

**DESIGN DEVELOPMENT** CPL DT

APPROVED

SCALE @ B1

DRAWING NO. IMHC-CV-DG-2791

